

REQUEST FOR BIDS
CALHOUN COUNTY ADMINISTRATION
DEPARTMENT OF PURCHASING

ROAD SURFACE IMPROVEMENTS; RFB#119-15.

The County of Calhoun is requesting written bids from qualified providers of HMA wedge, single chip seal, fog seal, and new pavement marking to provide road surface improvements to a designated area of 22 Mile Road and S. County Line Road located in Calhoun County.

This Request for Bids with all pages, documents, and attachments contained herein or subsequently added or made a part hereof, submitted as a fully and properly executed bid, shall constitute a contract between the County of Calhoun and the approved low bidder, as determined by the County when approved and accepted by the County of Calhoun.

Bids must be submitted in complete original form by mail or by messenger in a properly marked and sealed envelope to the following address: Purchasing Department, County Building, 315 West Green Street, Marshall, MI 49068, no later than *Thursday, June 18, 2015, @ 3:00PM LOCAL TIME*. The sealed envelope shall be clearly noted "**ROAD SURFACE IMPROVEMENTS – RFB #119-15.**"

Questions regarding this request should be directed to:

Leslie Obrig, Purchasing Coordinator
Calhoun County
315 West Green Street
Marshall, MI 49068-1585
(269) 781-0981
lobrig@calhouncountymi.gov

The County reserves the right to accept or reject any or all bids and to waive informalities and irregularities in bids or bidding procedures, and to accept any bid determined by the County to be in the best interest of the County, even though not the lowest bid or to negotiate further with one or more vendors submitting bids.

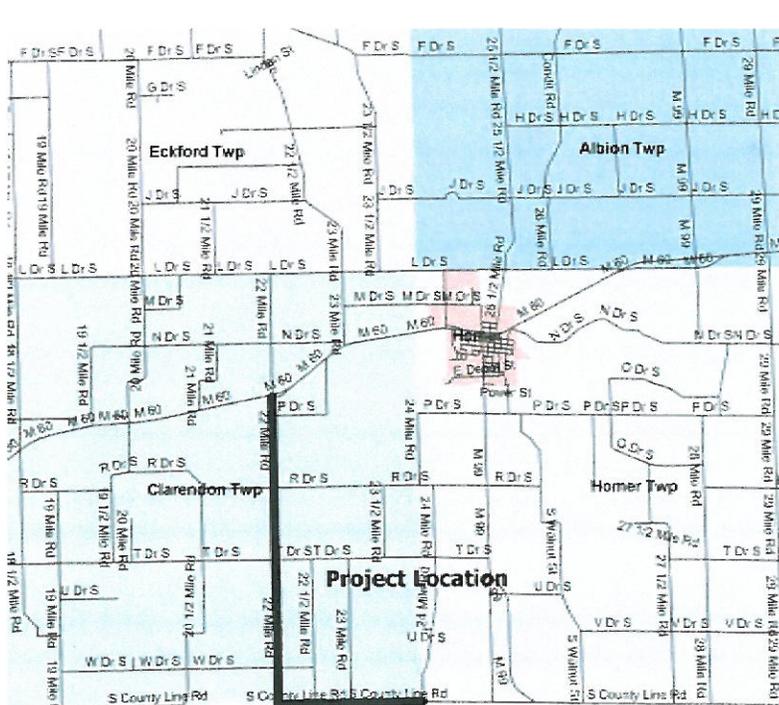
LATE BIDS WILL NOT BE ACCEPTED

CALHOUN COUNTY ROAD DEPARTMENT
 IN COOPERATION WITH
MICHIGAN DEPARTMENT OF TRANSPORTATION
 AND
THE FEDERAL HIGHWAY ADMINISTRATION

22 Mile Road from S County Line Road to M-60
 S County Line Road from 22 Mile Road to 24 Mile Road
 Clarendon and Homer Townships

Control Number: STL 13025

Job No.: 123888A



Average Daily Traffic
 22 Mile Road
 Present (2014) = 1482
 Future (2034) = 1901
 S County Line Road
 Present (2014) = 560
 Future (2034) = 682
 Posted Speed = 55 MPH
 Design Speed = 60 MPH

General Notes:

1. All materials and workmanship shall be in accordance with the Michigan Department of Transportation's 2012 Standard Specifications for Construction.
2. All Traffic Control Devices shall be furnished and operated in accordance with the 2011 Michigan Manual of Uniform Traffic Control Devices and Section 812 of the Michigan Department of Transportation's 2012 Standard Specification for Construction.
3. Improvements covered by these plans are in accordance with section D (PM) of the MDOT LAP Guidelines for Geometric Design dated March 4, 2014

Project Description: 6.308 miles

Contract for placing HMA wedge material as directed, placing a single chip seal, fog seal and new pavement markings.

Calhoun County Road Department Approval

 Christopher J. Bolt, Managing Director

 Date

 Angela N. Kline, P.E., County Highway Engineer

 Date

Special Details:

Temporary Traffic Control Devices WZD-125-E
 Ground Driven Sign Supports for Temp Signs WZD-100-A

**CALHOUN COUNTY ROAD DEPARTMENT
LOG PROJECT
22 MILE ROAD AND SOUTH COUNTY LINE ROAD**

CCRC: ANK

1 of 1

06/24/2014

PROJECT LOCATION

This project begins on 22 Mile Road at S. County Line Road, P.O.B. STA. 0+00 and continues north to M-60, P.O.E. STA. 227+52 and then on S. County Line Road beginning at 22 Mile Road, P.O.B. STA. 0+00 east to 24 Mile Road, P.O.B. STA. 105+55.

The Engineer will establish the location for all items of work.

PROJECT DESCRIPTION

The project provides for HMA wedging and chip seal with bituminous fog seal at the various project locations.

Permanent pavement markings shall be placed according to the layout provided.

SURFACING ITEM

In accordance with the typical cross sections and as directed by the Engineer. HMA 36A is included to be used for slope correction and deteriorated pavement areas in various places throughout the project as directed by the Engineer, paid for as HMA, 36A.

HMA, 36A	50	Ton
Seal, Single Chip, Modified	81,450	Syd
Bituminous Fog Seal	81,450	Syd

MAINTAINING TRAFFIC

Maintain traffic in accordance with special provision for traffic control and as directed by the Engineer.

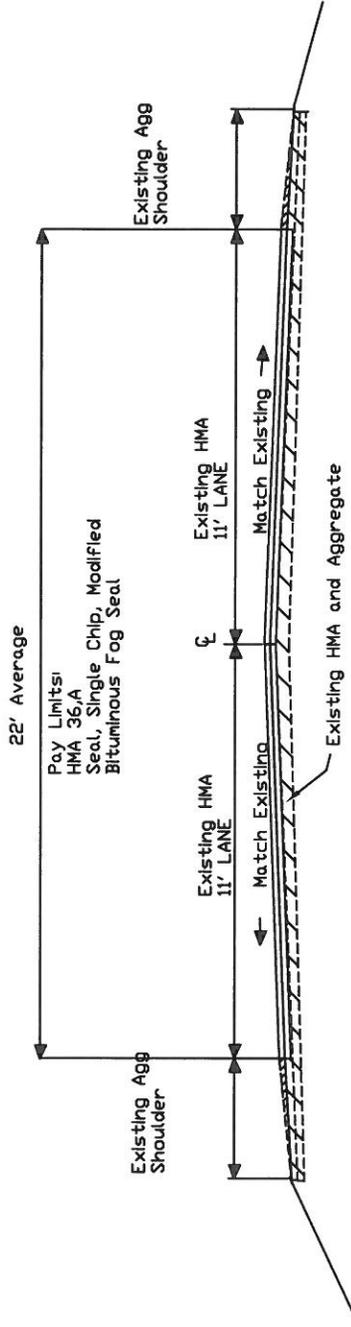
Lighted Arrow, Type C, Furn	2	Ea
Lighted Arrow, Type C, Oper	2	Ea
Minor Traf Devices	1	LS
Traffic Regulator Control	1	LS
Sign, Type B, Temp, Prismatic, Furn	300	Sft
Sign, Type B, Temp, Prismatic, Oper	300	Sft

PAVEMENT MARKING

Place centerline and edge white markings according to the layout provided.
Place edge whites providing an 11' driving lane.

Raised Pavt Marker, Temp, Type 1, Yellow, Bidirectional	334	Ea
Pavt Mrkg, Waterborne, 4 inch, White	66,614	Ft
Pavt Mrkg, Waterborne, 4 inch, Yellow	27,951	Ft

ROW 66' WIDTH



22' Average

Pay Limits:
HMA 36A
Seal, Single Chip, Modified
Bituminous Fog Seal

Existing Agg
Shoulder

Existing HMA
11' LANE

CL

Existing HMA
11' LANE

Existing Agg
Shoulder

Existing HMA and Aggregate

22 Mile Road
POB STA 0+00 to PDE STA 227+52

HMA APPLICATION TABLE

ITEM	RATE OF APPLICATION	PERFORM. GRADE	REMARKS
HMA 36A * Varies * Bituminous bod coat: SS1 max application rate= 0.10 Gal/Syd	165#/SYD*	PG 58-28	Wedge material



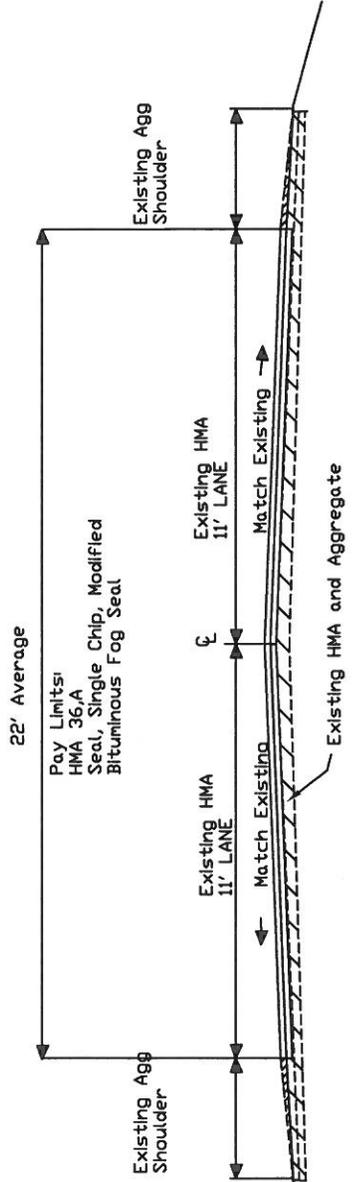
Old US 27
From S County Line Road
to M-60

JN:
123888A

SHEET NUMBER
SCALE
NO SCALE

DRAWN BY: ANK DATE: 06/25/2014

RDW 66' WIDTH



S County Line Road
POB STA 0+00 to POE STA 105+55

HMA APPLICATION TABLE

ITEM	RATE OF APPLICATION	PERFORM. GRADE	REMARKS
HMA 36A * Varies Bituminous bod coat; SS1 max application rate= 0.05 to 0.10 Gal/Syd	165#/SYD**	PG 58-28	Wedge material



S County Line Road
From 22 Mile Road
to 24 Mile Road

JN:
123888A

SHEET NUMBER	SCALE
NO SCALE	NO SCALE

DRAWN BY: ANK DATE: 06/25/2014

CALHOUN COUNTY ROAD DEPARTMENT

CCRC: ANK

1 of 1

05-07-15

PROGRESS CLAUSE: Work shall begin within 10 days of receiving Notice of Award or on a date agreed upon with the Engineer. In no case, shall any work be commenced prior to receipt of formal notice of award by the department.

This contract shall be completed by September 15, 2015.

The approved low bidder (s) for the work covered by this proposal will be required to participate in a pre-construction meeting with local agency owner representatives to work out a detailed progress schedule. The schedule for this meeting will be set within one week after the approved low bidder is determined.

The named subcontractor (s) for Designed and/or Specialty Items, as shown in the proposal, is recommended to be at the scheduled meeting if such items materially affect the work schedule.

The Calhoun County Road Department will arrange the time and place for the meeting.

The progress schedule shall include, as a minimum, controlling work items for the completion of the project and the planned dates (or work day for a work day project) that these work items will be controlling operations. When specified in the bidding proposal, the date the project is to be opened to traffic as well as the final project completion date shall also be included in the project schedule.

If the bidding proposal specifies other controlling dates, these shall also be included in the progress schedule.

Liquidated damages will be assessed according to Section 108 of the 2012 Michigan Department of Transportation Standard Specifications for Construction.

CALHOUN COUNTY ROAD DEPARTMENT

**SPECIAL PROVISION
FOR
MAINTAINING TRAFFIC,
PAVEMENT MARKING & SIGNAGE**

CCRD: ANK

1 of 1

06-24-14

General.- Traffic shall be maintained at all times during construction in accordance with Section 104.07C, 104.11, Section 812 and 922 of the *2012 Standard Specifications for Construction*, the Michigan Manual of Uniform Traffic Control Devices (MMUTCD) and as specified herein.

Construction Influence Area (CIA).- The CIA shall include the project road rights-of-way and 200 feet in all directions on side streets. In addition the CIA shall include limits of all construction signing.

Traffic Control Devices & Restrictions-

All traffic control devices and their usage must conform to Part 6 of the MMUTCD. The Contractor must review, adjust and maintain all traffic control devices on a daily basis or as directed by the Engineer. The Contractor must notify the Engineer at least 24 hours in advance of erecting construction zone signing. Access to all businesses and residences must be maintained at all times and work must be coordinated with the Engineer.

The Contractor must maintain all traffic control devices and is responsible for reviewing the adequacy and condition of all traffic control devices at least once per day for the duration of the project. Replacement and repair of the traffic control devices will be restricted to daylight hours. All signs damaged as a result of the Contractor's operations must be replaced by the Contractor at their expense.

Measurement and Payment.-

Items necessary for maintaining traffic being measured and paid for separately include:

<u>Pay Item</u>	<u>Pay Unit</u>
Traffic Regulator Control	Lump Sum
Minor Traf Devices	Lump Sum
Lighted Arrow, Type C, Furn	Each
Lighted Arrow, Type C, Oper	Each
Plastic Drum, High Intensity, Furn	Each
Plastic Drum, High Intensity, Oper	Each
Sign, Type B, Temp, Prismatic, Furn	Square Foot
Sign, Type B, Temp, Prismatic, Furn	Square Foot

Signing for traffic regulator operation, shall be in accordance with the Michigan Department of Transportation maintaining traffic typical drawings WZD-125E, M0020a & M0140a. Estimated quantities for maintaining traffic are based on one (1) lane closed with traffic regulator control and Road Work Ahead (W20-1) signs on intersecting roads. Pavement markings on the project shall be Pavement Marking Waterborne, 4 inch White and yellow. The contractor may use cones in tangent sections.

CALHOUN COUNTY ROAD DEPARTMENT

SPECIAL PROVISION
FOR
SEAL, SINGLE CHIP, MODIFIED

CCRD: ANK

1 of 7

05/06/2015

a. Description. Prepare the pavement surface and apply materials per Section 505 of the 2012 Michigan Department of Transportation Standard Specifications for Construction and the following specifications enclosed within this special provision.

b. Materials. Use coarse aggregate 29A meeting the specifications per Section 902 of the 2012 MDOT Standard Specifications for Construction.

HFRS-2M emulsion shall be provided per the specifications. The Contractor shall be responsible for proper heating of the asphalt, needed storage facilities, and for all truck demurrage charges that are incurred.

Raised Pavt Marker, Temp shall be “Yellow, Type 1, Bidirectional” (reflective on both sides). See Section H1.

c. Construction.

A. Equipment. Equipment must be safe, environmentally acceptable, and able to produce a quality product.

1. **Pressure Distributor.** The pressure distributor shall have a computerized application rate and speed control. This control shall have a radar ground sensing device that controls the application rate regardless of ground speed or spray bar width. The pressure distributor shall be capable of maintaining the asphalt at the specified temperature. The spray bar nozzles shall produce a uniform triple lap application fan spray, and the shutoff shall be instantaneous, with no dripping. Each pressure distributor shall be capable of maintaining the specified application rate within +/- 0.015 gallons per square yard for each load.
2. **Chip Spreader.** The chip spreader shall be self-propelled, equipped with pneumatic tires and have a screen to remove oversized material.
3. **Compacting Equipment.** Use a self-propelled, pneumatic tired roller, weighing not less than 8 tons.

4. **Brooms.** Provide motorized brooming equipment, capable of cleaning the road surface before treatment & removing loose particles after treatment. Provide pick-up sweepers to clean road surfaces adjacent to lawns or roadways with curb & gutter.
5. **Lights on Equipment.** Equip all self-propelled equipment with a minimum of one flashing, rotating or oscillating amber light, visible to traffic in all directions. Equip chip spreaders with one such light on each side of the spreader.
6. **Miscellaneous.** Sufficient equipment (hand tools, thermometers, etc.) shall be provided to haul cover material to the job site. This is necessary to ensure continuous covering of bituminous material.

B. **Pre-Paving Meeting.** A pre-paving meeting will be held on site with the Engineer before beginning work to discuss the following:

1. Work schedule.
2. Traffic control plan.
3. Equipment calibration and adjustments.
4. Condition of materials and equipment, including transport units.
5. Mix design(s) including job-mix-formula (JMF); aggregate gradation; application rate of asphalt emulsion and aggregate (by stationing and course).
6. Contractor's quality control plan (method of yield check, etc.).

C. **Weather and Seasonal Limitations.**

1. Place asphalt when the pavement and atmospheric temperatures are at or above 55 °F; except do not place asphalt when the existing pavement temperature is 130 °F or above.
2. Do not place asphalt if there is threatening weather or if temperatures are forecast to be below 40 °F within 24 hours from the time of application.
3. Chip seal applications may only occur between the dates of May 15 and September 15.

D. General Placement Operation.

1. Log all existing pavement markings, including passing zones. Perform all surface preparations that may affect the performance of the application. Remove all cold plastic pavement markings and legends using an abrasion method. Remove markings just before the surfacing operation. Clean the pavement with a motorized power broom to remove all loose material. Clean all depressions not reached by the power broom using a hand broom. Thoroughly clean the outer edges of the pavement or shoulder. Extend cleaning one foot onto the adjacent paved shoulder where applicable.
2. Keep all vehicles and equipment involved in the chip sealing operation as close to each other as practical. Keep the asphalt emulsion distributor within 150 feet of the chip spreader. Do not place cover aggregate on asphalt that has been allowed to cool.
3. Locate longitudinal construction joints on a painted lane line or at the outside edge of the shoulder.
4. Perform rolling immediately after placing the aggregate and before the asphalt has cooled. Make a minimum of two complete passes over the aggregate. A complete pass is one trip, forward and backward, over the same path. Overlap each pass by one-half the width of the roller. Use a minimum of two rollers and proceed in a longitudinal direction at a speed not greater than 5 (five) mph. This speed will be strictly enforced.
5. Sufficient rolling shall be done to embed the cover material in the bituminous material.
6. Perform sweeping of the completed chip seal to remove excess loose aggregate within 24 hours with approval of the Engineer. Removal of aggregate in curb and gutter areas shall be done with an approved vacuum/pickup type sweeper. Sweep beyond the edge of pavement to help prevent migration of loose aggregate back onto the pavement. Do not sweep loose aggregate into lawns, curbed areas, or intersections. Failure to comply with these requirements will result in the withholding of payment.
7. Before opening to traffic, place W8-7 (LOOSE GRAVEL) signs with 35 mph speed plaques mounted below. Place these signs throughout the completed work, beginning 50 feet before the treated area in the direction of oncoming traffic at a maximum spacing of 1.0 mile, or as directed by the Engineer.
8. Before beginning the chip seal operation, protect all utility castings and raised pavement markers using tarpaper or other approved materials. Remove these protective coverings before sweeping and opening to traffic.

9. Allow the new surface sufficient cure time to prevent damage by vehicle tires before opening to traffic. Protect the new surface from potential damage at intersections and driveways as needed. The Contractor shall be responsible to repair any damages caused by traffic at its expense. The Contractor may want to consider using a pilot car system to protect the pavement surface. If this is done, the pilot car should maintain a maximum speed of 35 MPH to control the speed of following vehicles.

E. Application Rates.

Apply asphalt emulsion at a temperature between 170 and 190 °F, followed by a uniform application of aggregate material.

Notify the Engineer immediately if the specified target rates for either emulsion or aggregate are not at optimum application rates due to aggregate gradation or existing surface conditions. Document the new JMF (job mix formula) rate(s) by stationing if this occurs.

1. Asphalt Emulsion--The Contractor will calibrate their pressure distributor to achieve a surface application rate of 0.41 – 0.48 gallons per square yard. 0.45 gallons per square yard will be used for the JMF.
2. Coarse Aggregate--Apply within the range of 16-19 pounds per square yard. Eighteen (18) pounds per square yard will be used for the JMF.

F. Quality Control.

Take the following minimum measures to maintain quality control and uniformity. If quality control test results exceed any of the specified quality control tolerances, stop placement and immediately notify the Engineer. Identify the cause and document, in detail, the corrective action necessary to bring the operation into compliance. Secure the Engineer's approval before resuming work.

1. Asphalt. Determine the actual application rate by a 1000-foot yield check. Do not exceed a tolerance of ± 0.01 gallons per square yard from the established JMF.
3. Aggregate Material. Determine the actual application rate by a weighing with a one square yard tarp. Do not exceed a tolerance of ± 1.0 pounds per square yard from the established JMF application rate. Have a certified aggregate technician collect a sample from the project aggregate stockpile once every 500 tons or a minimum of once per day's production, and perform a sieve analysis. This analysis must meet the requirements of

section 902 and be within the quality control tolerances of Table 2 from the established JMF.

Table 2: Chip Seal Aggregate Quality Control Tolerances

Sieve Size	3/8"	#4	#8
Tolerance (+/-)	5.0%	5.0%	3.0%

- G. **Stockpiling Aggregate Materials.** The Contractor shall secure its stockpile areas at locations near the worksite. The Contractor shall be required to provide written approval from the property owner to the Engineer.
- H. **Maintenance of Traffic.** The Contractor shall provide sufficient traffic regulators to maintain traffic through the project construction area. The Contractor shall follow the requirements of the latest edition of the Michigan Manual of Uniform Traffic Control Device (MMUTCD), and the enclosed Special Provision for Maintaining Traffic. On heavily traveled primary roads, the contractor may elect to use a pilot car as noted in Section A.

Temporary Raised Pavement Markers. Raised, Pavt Marker, Temp, Type 1, Yellow, Bidirectional shall be placed along the centerline, prior to application of aggregate, at intervals specified below:

- a. Markers shall be placed at intervals of 50 feet on tangent sections of roadway or gentle curvatures as approved by the Engineer.
- b. Markers shall be placed at intervals of 25 feet on tight curvatures of roadways or as directed by the Engineer.

Temporary markers are not to be removed by the Contractor.

I. **Documentation.**

1. Provide the Engineer a daily report with the following information:
 - a. Control section, project number, county, route, Engineer
 - b. Date, air temperature, pavement temperature, humidity
 - c. Asphalt emulsion temperature
 - d. Beginning and ending stations (placement and brooming/vacuuming)
 - e. JMF including gradation and application rates per course (aggregate and asphalt)
 - f. Yield checks on asphalt (three per day, per course, minimum and average.)
 - g. Yield checks on aggregate material (three per day).

- h. Aggregate gradation and moisture content (one per day per type, minimum)
 - i. Length, width, total square yards
 - j. Quantity of traffic control devices including loose gravel signs with 35 mph speed plaques
 - k. Contractor's signature
2. Provide the following additional materials documentation as directed by the Engineer:
- a. Aggregate certification or Shipment of Tested Stock Report (Form 1900)
 - b. Asphalt-per current acceptance procedures
 - c. Bill of ladings or delivery tickets for coarse aggregates and asphalt
 - d. Aggregate Wear Index
- J. **Delayed Acceptance.** A minimum of 30 days after placement, the Engineer will inspect the project with the Contractor for surface flushing, surface patterns or loss of stone. If these deficiencies are found, corrective work will be required as directed by the Engineer. Complete all corrective work within seven working days of the review, or by an agreed upon date. All costs associated with completing this corrective work, to the satisfaction of the Engineer, will be borne by the Contractor.
- d. **Measurement and Payment.** The completed work for Seal, Single Chip, Modified will be paid for at the contract unit price for the following:

Contract Item (Pay Item)	Pay Unit
Seal, Single Chip, Modified.....	Square yard

Seal, Single Chip, Modified will be measured in place by the unit square yard and paid for at the contract unit price per square yard, which price shall be payment in full for all labor, materials, and equipment needed to accomplish this work.

Seal, Single Chip, Modified includes placing asphalt emulsion and aggregate material to a pavement as specified in this special provision. Payment includes all material sampling and testing, placement and removal of tarpaper over utility covers, surface preparation, brooming and vacuuming, project documentation, and any traffic control where the Contractor elects to use a pilot car system (including the G20-4 sign). Temporary traffic signs, mobile attenuators, lighted arrows, temporary raised pavement markers or minor traffic devices shall be paid for separately.

Adjustments in the unit price will be made when the approved application rates for asphalt and aggregate material are outside the ranges specified in subsection (E) of this special provision.

CALHOUN COUNTY ROAD DEPARTMENT

SPECIAL PROVISION FOR BITUMINOUS FOG SEAL

CCRD: ANK

1 of 2

07/15/2013

a. Description: This work shall consist of all labor, materials, and equipment required to apply a bituminous fog seal as specified in this special provision.

b. Materials: All equipment must meet the requirements under Section 505 of MDOT's 2012 Standard Specifications for Construction, Section 904, except as modified herein:

Pressure Distributor:

The pressure distributor shall have a computerized application rate and speed control device interconnected with the asphalt emulsion pump such that the specified application rate will be supplied at any speed. This control shall have a radar ground sensing device that controls the application rate regardless of ground speed or spray bar width. The pressure distributor shall be capable of maintaining the asphalt emulsion at the specified temperature. The spray bar nozzles shall produce a uniform fan spray, and the shutoff shall be instantaneous with no dripping. Each pressure distributor shall be capable of maintaining the specified rate of application within +/- 0.015 gallons per square yard for each load.

Broom/Sweeper:

The use of a rotary-powered broom is required to remove the loose material from the surface to be treated and for removing loose aggregate after the work has been completed.

Miscellaneous:

Provide all equipment including hand tools, thermometers, etc. Equip all self-propelled equipment with at least one approved, flashing, rotating or oscillating amber light, visible to traffic in all directions.

c. Construction CSS-1h (at 50% dilution) shall be spread at a target rate between 0.10 and 0.15 gallons per square yard with a target rate of 0.12 gallons per square yard.

If the target rate of 0.12 gallons per square yard is not the optimum application rate due to the gradation of the coarse aggregate or due to existing surface conditions of the pavement, the contractor shall notify the Road Commission's inspector immediately and prior to application. Upon approval of changes by the engineer, the contractor shall then document the new JMF rate(s) by stationing. All truck demurrage will be the responsibility of the contractor.

Fog Seal shall be placed 24-48 hours after completion of the chip seal application and brooming.

d. Measurement and Payment: The completed work for Bituminous Fog Seal will be paid for at the contract unit price for the following:

Contract Item (Pay Item)

Pay Unit

Bituminous Fog Seal.....Square Yard

Payment for **Bituminous Fog Seal** will be measured in place by the unit square yard and paid for at the contract unit price per square yard, which includes all equipment, labor and materials needed for surface preparation and the bituminous emulsion concentrate. Water added to bituminous concentrate will be included for measurement in payment.

No adjustments in the unit price will be made for approved rate of asphalt emulsion that is within the ranges specified herein.

**CALHOUN COUNTY ROAD DEPARTMENT
SPECIAL PROVISION
FOR
HMA APPLICATION ESTIMATE**

CCRD: AT:ANK

1 of 1

06-24-14

Description.- This work shall be done in accordance with the requirements of Division 5 of the *2012 Standard Specifications for Construction*, except as herein specified.

Construction Method- The work shall include furnishing and placing HMA on deteriorated areas before chip seal activities begin. The asphalt wedging shall be placed with an asphalt paver and raked along the inner edges for proper ride transition.

Materials.-

The HMA, 36A shall be used for asphalt wedging and shall vary up to a maximum yield of 165 pounds per square yard. If additional thickness is required it shall be placed in multiple lifts.

The Performance Grade asphalt binder range for the resurfacing shall be 58-28.

The Aggregate Wear Index (AWI) shall be a minimum of 260 for the Top course.

The HMA Bond Coat material shall be per Section 501.03.D of the MDOT *2012 Standard Specifications for Construction*. The uniform rate of application shall be 0.05 to 0.10 gallons per square yard. No separate payment shall be made for the HMA Bond Coat.

Measurement and Payment.- Measurement and Payment shall be at the contract unit price per ton.

CALHOUN COUNTY ROAD DEPARTMENT

Notice To Bidders

UTILITY COORDINATION

CCRD: ANK

1 of 1

06/24/2014

The contractor shall cooperate and coordinate construction activities with the owners of utilities as stated in Section 104.08 of the 2012 MDOT Standard Specifications for Construction. In addition, for the protection of underground utilities, the contractor shall follow the requirements in Section 107.12 of the 2012 MDOT Standard Specifications for Construction. Contractor delay claims, resulting from a utility, will be determined based upon Section 109.03 of the 2012 MDOT Standard Specifications for Construction.

PUBLIC UTILITIES

The following Public Utilities have facilities located within the Right-of-Way:

Lacy Johnson
AT&T
2919 Millcork, Room 101
Kalamazoo, MI 49001
(269) 384-4472

Mr. Bob Stinnett
Consumers Energy Company
311 East Michigan
Battle Creek, MI 49017
(269) 969-8687
Fax: (269) 969-8549

Joe Schopf
Comcast
11921 E M-89
Richland, MI 49083
(269) 203-7114

Mr. Jack Gwilt
Semco Energy
Battle Creek, MI 49017
(269) 966-0404
Fax: (269) 966-0428

The owners of existing service facilities that are within grading or structure limits will move them to locations designated by the Engineer or will remove them entirely from the highway Right-of- Way. Owners of Public Utilities will not be required by the County to move additional poles or structures in order to facilitate the operation of construction equipment unless it is determined by the Engineer that such poles or structures constitute a hazard to the public or are extraordinarily dangerous to Contractor's operations.